

AUTOMOTIVE
SECTIONTRUCKS MAY SOLVE
PROBLEM OF HOMES

Homeless Americans—thousands of them in nearly every part of this country—are walking the streets, eagerly watching for a vacant house or even a few rooms in which they may live. As a result of this condition we are witnessing one of the most staggering building programs in the history of this country—a program that is compelling the contractors to use every known device that makes for efficiency in building and one in which the motor truck looms up as the greatest aid in the transportation of this vast volume of building materials.

The homeless, in their mad desire to get a home, or a room, are resorting to every known method that will bring results. They are bribing rental agents and offering fabulous rents for any kind of a dwelling that affords shelter.

The appeal of these homeless people is becoming almost as pathetic as those in the war torn countries. The seriousness of this problem can easily be visualized when we see people living in tents, houseboats, and practically every imaginable shanty that affords shelter from the sun and rain. This condition requires the immediate erection of thousands of homes and buildings. To do this, and effectively meet the on-rush of building progress, it is no longer safe for the contractors to rely upon the horse-drawn vehicle for their delivery of materials, for blaming the delays on old methods is not a very acceptable excuse when the men who let out these contracts know that a fleet of good motor trucks will fulfill this demand for quick deliveries day after day.

If more homes and buildings must be built each day, the excavating must be carried out faster—more brick—more steel—more lumber and Think it over."

plaster must be delivered each day, otherwise this task will not be accomplished; but the contractor can overcome this situation by placing good, reliable motor trucks to work on his delivery equipment. Instead of wasting time with the old methods, some of the most successful building contractors in the country have been farighted enough to see this condition. They have availed themselves of the more efficient delivery utility and now their motor trucks haul the steel, the brick, the tile, the cement, the lumber, and the plaster from which the homes, factories, offices and buildings are fashioned. Look at their delivery equipment and you will find that it ranges from a light truck for small fixtures to the ponderous dump body type for the giant load.

Having decided that the motor truck is the logical servant to meet this bigger, quicker and more efficient delivery demand, the next problem is the selection of bodies that will be of the greatest value to the contractor. One of the Pilgrimage dealers who is in touch with all the building contractors in a large city, and makes a study of their needs, has this to say regarding the most serviceable type of body for construction work:

"The contractors are making a big demand for 3½ ton trucks equipped with the hot type dump bodies, as they are realizing that this type of body permits a much wider use in building construction work, because it is adapted to almost any kind of building materials."

SAYS POOR ROADS MAKE
THE LONGEST ROUTES

"A good road is the shortest route between starting point and destination," says G. C. Frey, of the Kissel Motor Car Co.

"Likewise, a poor road is the longest. If motor-truck owners don't believe it, figure up the difference in time, labor, gas, oil, and last, but far from least, depreciation on the truck, after covering both routes.

"The result you get is the best reason why poor roads are our greatest liability and likewise why good roads will prove our greatest asset. Think it over."

BULLOCK SOLVES TRUCK DELIVERIES

FLEETS OF TRUCKS
TO DELIVER COAL

By WARREN B. BULLOCK
(Formerly Captain, Motor Transport Corps, U. S. A.)

One of the simplest problems in truck operation is the handling of coal by motor, for the delivery of fuel comprises fewer problems than almost any particular line of truck operation, because of the narrow variation of deliveries.

A miscellaneous delivery business may have a load of light weight bulky materials one day, and the next be required to handle a big shipment of machinery, heavy metallic cargo of little bulk. The coal delivery problem, however, is confined to the best manner of distribution of various sized orders of a standard weight.

The average coal truck fleet needs to comprise few different types of vehicles. Into the coal delivery problem, however, comes the utilization of the automatic dump truck, with a need for a higher capacity body than would be required for a miscellaneous delivery body.

Bulk of Deliveries Standard.
The bulk of the shipments of a coal delivery fleet are with a standard three-ton truck, with additional five-ton machines for handling deliveries to big consumers, and the five-ton trucks are needed usually as automatic dump trucks. There are few times when it will be found advisable for a five-ton truck to be used without the dump attachment, and the best method is to handle a fleet with all of the five-ton trucks of the automatic dump type.

There are comparatively few deliveries to make where the total load is less than two tons. At times there may be an emergency delivery of a small quantity, but this is seldom the case for the larger bulk of a coal company's clients.

A Typical Fleet.
In one of the big cities of the United States a typically well operated coal truck fleet is balanced about as follows:

One and one-half ton trucks..... 2
Three-ton trucks..... 2
Five-ton trucks..... 4
All of the five-ton trucks are of the automatic dump type, and the three-ton trucks are generally half dump and half solid body. The replacements of the three-ton trucks, however, have all been of the dump type, on the theory that when it is possible to use the dump apparatus the time saved in cost of labor of coal handling is so great as to much more than balance up, in the long run, the additional cost of the power-hoisting machinery.

About Forty Trucks.
This fleet is comprised of about forty trucks. In other words, with two of the one and one-half ton trucks the company finds that it is covering one-quarter of its total business, the two trucks being the ratio to the total in the fleet. Half of the company's business is with the three-ton type of trucks, the balance being distributed between the light and the heavy models for small retail and the bigger buyers.

The company, however, is often forced to throw its three-ton trucks into the five-ton field, and then uses the dump type exclusively for quick handling of big shipments.

The ton and a half truck is used for the small consumer, whose cash on hand is not sufficient to enable him to buy his winter's supply of coal at one time, and whose fuel bin capacity may even if he has the money be insufficient to care for his needs. The coal company finds in the city under question probably a smaller proportion than the average city of householders who have to do business in small lots. In addition,

the small lot business is in large measure handled by the small jobbers, who act as agents for the company in question.

Removable Partitions.

In case, however, there is a sudden demand of a large number of small shippers, the company's fleet is so built with removable partitions like ship bulkheads, which will divide the three-ton truck's capacity into two or three sections, making three deliveries to three addresses possible with a single load. These bulkheads, also are used when a consumer buys a quantity of two different types of coal, as for furnace banking or kitchen range.

The company's statistics on saving in coal delivery costs by the installation of motorized equipment are not available to the ordinary public. It can be said, however, that the motorization of the delivery department made such a saving in labor costs and time of delivery, that the fleet practically paid for itself the first year.

This was a case where the fleet was installed in big units, the first contract being for fifteen trucks and the experience of one year resulted in the sale the following year of practically all the company's old horse-drawn wagons.

There are two kinds of dollars—one that is never worth more than a hundred cents, and one that grows in value. When you put your money in War Savings Stamps, you change your hundred-cent dollars into the kind that grow.

MOTOR TRUCK KING
IN DISTRICT TODAY

Washington is in line with our other leading cities in crowning the motor truck king of the highways, and today the people of this city will turn out to witness one of the most interesting parades that has ever passed through its streets.

Motor truck dealers of the Federal Capital have perfected plans, which will present the latest utility for transporting our commerce from the source to the point of consumption, according to F. W. Penn, secretary of the motor truck committee of the Automobile Chamber of Commerce.

The various departments of the United States Government have arranged to enter hundreds of motor trucks, while new vehicles of every design and old veterans that have done duty in long-distance transportation will present a contrast which will be interesting.

Huge Army Trucks.

There will be huge army trucks, like the ones which carried our boys and our ordinance supplies to the battle trenches in time to stem the incoming horde of Germans—motor trucks which carry our mail into the rural districts, remote from railways, and great numbers of them, with all kinds and types of bodies, which have

carried ore from the mines to the steel mill, and from the steel mill door to the points of utilization and consumption.

Then there will be trucks which are filling that all-important duty of carrying foodstuffs from the farm to the market. The trucks are in a service which proved so important during the war when, with the railroads overburdened, they saved the situation and brought milk and other perishable foodstuffs in great bulk into the cities of Baltimore and Washington, so that the added masses of people in the Capital district might have food.

Then, again, trucks with bodies for carrying live stock will be in line. Through this type of truck the stock yards of the country are receiving their great bulk of cattle, hogs and sheep. For instance, to show you how this form of transportation is growing, there were shipped into the Omaha stock yards in the first nine months of 1917, 52,129 hogs. For the same period in 1918 this had increased to 142,220 hogs, an increase of 90 per cent. From July to September, 1917, there were shipped 12,189 head of cattle, while this had increased to 17,681 for the same period the year following.

The Cincinnati, Indianapolis, Louisville and Kansas City markets receive a large proportion of their hogs by motor trucks.

The economy comes when one truck hauls one or more trailers. Recently one motor truck, running from Dallas, Tex., to Farmersville, hauling four trailers, carried seventy head of sheep, seventeen head of steers—net weight 29,500 pounds—over a distance of thirty miles, twenty-two miles of which was over black land roads.

SAYS PEACE WILL
BOOM AUTO SALES

That the signing of the peace treaty will cause increased buying of motor trucks, is the opinion of W. L. Kissel, secretary and treasurer of the Kissel Motor Car Company.

"Notwithstanding the deluge of business that came to American industries with the signing of the armistice," he says "I am firmly of the opinion that when the peace treaty is actually signed there will be a further increase in orders placed in America, which will make it necessary for our industries, manufacturers and other producers, to increase their transportation equipment to keep up with their increased production activities."

"We must not forget that there are at least three different stages to the reconstruction period in Europe."

Building up of the homes, streets, etc., which has been going on since November last. This is only the smallest part of reconstruction work. There will be the rebuilding of the factories and supplying equipment necessary to put these factories on a production basis. Here is a large market for American made supplies, materials, and equipment. When these factories

are built and equipment installed, it will again be up to America to furnish supplies and material, raw and otherwise, for these factories. "The last two stages in the reconstruction period will have to await the signing of the peace treaty before actual work can be started."

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WASHINGTON, D. C.

June 3, 1919.

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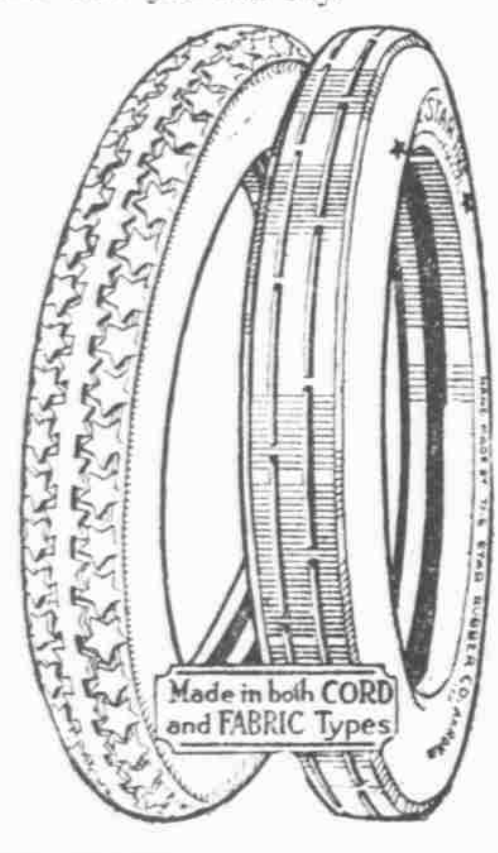
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